



Planning Committee
20th June 2023

Application No	:	22/01723/FUL
Location	:	The Avenue Springfield And Land Forming Part Of Beaulieu Parks Sports Ground Springfield Chelmsford Essex
Proposal	:	Construction of a two-way bus link and pedestrian and cycle paths to connect the Beaulieu Park Railway Station to White Hart Lane with utility, landscape and infrastructure works, together with associated and ancillary development
Applicant	:	Countryside Zest (Beaulieu Park) (LLP)
Agent	:	DWD
Date Valid	:	14th September 2022
Development Type	:	Large Major All Other (D06)
Target Date	:	13th December 2022
Consult Expiry	:	5th April 2023

Contents

1. Executive Summary	1
2. Description of Site	2
3. Details of the Proposal	2
4. Other Relevant Applications.....	3
5. Summary of Consultations	4
6. Planning Considerations.....	4
7. Community Infrastructure Levy (CIL) & s106 Agreement	13
8. Conclusion.....	13

Appendices:

Appendix 1	Consultations
Appendix 2	Drawings

1. Executive Summary

- 1.1. The application is referred to Planning Committee as it includes CCC owned land and an adverse third party representation has been received.
- 1.2. Beaulieu Park Railway Station forms the cornerstone of a series of sustainable transport measures for North-East Chelmsford; following reserved matters approvals, work commenced on the station at the end of 2022. The application brings forward the requirement for a two-way bus link

road from White Hart Lane to Beaulieu Park Railway Station as set out within the s106 Agreement forming part of the outline planning permission for Beaulieu; 09/01314/EIA refers.

- 1.3. The bus link road together with the associated footway and shared footpath/cyclepath have been the subject of extensive pre-application discussion and negotiation. The scheme design responds acceptably to the heritage sensitivity of the site, acknowledges existing below and above ground constraints, meets highway technical and design standards and will deliver a direct bus route and safe, convenient, accessible and well-lit footpaths/cyclepaths to the future railway station. The scheme forms a key component of the overall sustainable transport strategy for Beaulieu Park Railway Station.
- 1.4. Approval is recommended subject to the completion of a s106 Agreement to secure a commuted sum for maintenance of the shared footpath/cyclepath and its associated lighting and the grass verge alongside the southern side of the bus link road by the City Council.

2. Description of Site

- 2.1. The application relates to an existing access road, The Avenue, which runs parallel with the main Great Eastern railway line from London Liverpool Street to Norwich and provides an existing one-way access to New Hall School from White Hart Lane. The road is bounded to the north by Beaulieu Park Recreation Ground, its associated buildings and car parking. The Beaulieu Park residential neighbourhood lies beyond the recreation ground. An existing gravel path which wraps around the perimeter of the recreation ground and is fronted by properties in Burnell Gate and Wharton Drive, forms part of the application.
- 2.2. An area of Estate Parkland, which forms part of a series of compensatory measures to offset the acknowledged harm to the Grade I listed building at New Hall and its Grade II Registered Park and Garden arising from the Beaulieu development, lies to the north-east of the site.
- 2.3. An existing property, the Gate Lodge is located along The Avenue, adjacent the railway line and a short distance from the entrance onto White Hart Lane.

3. Details of the Proposal

- 3.1. The application seeks to provide a two-way bus link to the future Beaulieu Park Railway from White Hart Lane. The scheme would involve altering and widening the existing access road between White Hart Lane and The Avenue to a 6.5m carriageway. The bus link would connect at its eastern end with the approved two-way bus link being delivered as part of the Beaulieu Park Railway Station development. The widened route would continue to provide one-way access to New Hall School from White Hart Lane.
- 3.2. The existing path which wraps around the perimeter of the Beaulieu Park Recreation Ground and is fronted by residential properties in Wharton Drive and Burnell Gate would be upgraded and widened to deliver a 3.5m shared footpath/cyclepath. The path would connect with a future primary footpath/cyclepath wrapping around the edge of the Estate Parkland West and run parallel to the bus link, crossing The Avenue to connect with the approved footpath/cyclepath being delivered as part of the reserved matters approval for Beaulieu Park Railway Station (22/00473/REM refers). The crossing of the Avenue has been designed to give pedestrians and cyclists a right of way. The shared footpath/cyclepath would be lit by timber bollard lighting and existing column lighting.

- 3.3. A 1.2m wide footway is proposed to run alongside the initial section of the bus link road, from the existing footpath on White Hart Lane, parallel to Beaulieu Park Recreation Ground, up to the point where it would meet with the shared primary footpath/cyclepath at the edge of the playing fields. Column lighting is proposed along the length of the bus link road.
- 3.4. The works to widen the existing junction on White Hart Lane require removal and replacement of the existing wall, pier and sign on the north-western side of the road. The wall, pier and sign mark the entrance to New Hall School.
- 3.5. Compensatory landscaping is proposed alongside the bus link road.
- 3.6. The existing access and parking arrangements for Beaulieu Park Recreation Ground would be unaffected by the application.
- 3.7. The application has been the subject of extensive pre-application discussion between the local planning authority, the Council's Heritage Officer, the Parks & Green Spaces team, Historic England, the Highway Authority and the applicant. An on-site meeting also took place with Cadent Gas.
- 3.8. The applicant, the local planning and highway authorities and Network Rail met with New Hall School in November 2021 when the proposed development was discussed. Further meetings have taken place / correspondence has been entered into with the school during the lifetime of the application.

4. Other Relevant Applications

- 4.1. Outline planning permission was granted for the mixed-use residential-led development at Beaulieu in March 2014; 09/01314/EIA refers.
- 4.2. Outline planning permission was granted for a new railway station and related development together with associated access, parking and landscaping in May 2013; 10/00021/EIA refers.
- 4.3. Reserved matters for Beaulieu Park Railway Station were approved in two phases; 21/01097/REM (Phase 1) and 22/00473/REM (Phase 2) refers. Work has since commenced on the construction of the station. A separate full planning application, termed the 'Parkland Edge', sought planning permission for landscape woodland buffer planting on land between New Hall Lane and the Estate Parkland to the north, and the station site to the south; 21/02485/FUL refers. The planning permission secured an area of additional woodland planting to the north of the station, which together with the planting within the station site itself, would deliver a comprehensive landscape strategy, which would protect, and enhance, the Grade II Registered Park and Garden.
- 4.4. The station scheme connects with:
 - A future bus link from White Hart Lane (the subject of this planning application).
 - Future strategic foot and cyclepaths to the west serving Beaulieu and Beaulieu Park and providing connections to existing controlled pedestrian crossings of White Hart Lane (the subject of this planning application).
 - A future section of estate road linking roundabout 5 of the RDR with the station and which would be provided with off-carriageway strategic footpaths/cyclepaths to either side. Full details of the estate road would form part of a future planning application to include for the future widening of a section of Generals Lane leading to New Hall Lane (planning application 23/00380/FUL refers – currently pending consideration).

- A future off-carriageway strategic primary footpath/cyclepath to run alongside Generals Lane, providing access to the station from the northern parts of the development and future areas of the Chelmsford Garden Community (planning application 23/00380/FUL refers – currently pending consideration).

The key pieces of key infrastructure, listed above, are to be delivered by the applicant and have been the subject of detailed discussion with Network Rail, CCC and ECC, to ensure proper integration with the station scheme.

5. Summary of consultations

- Boreham Parish Council – No objections.
- Springfield Parish Council – No objections.

CCC Parks & Open Spaces – No objections subject to the provision of timber bollards on the roadside edge of the upgraded footpath/cyclepath as a defensive means of preventing parking.

- CCC Property Services – No comments.
- CCC Public Health & Protection Services – Air quality matters addressed; no further comments.
- Essex County Council Highways – Acceptable; subject to conditions and informatives.
- Essex County Council (SUDS) – Acceptable; subject to conditions and informatives.
- Essex County Council Historic Environment Branch – No archaeological recommendation.
- National Highways – No objections.
- Historic England – Defer to specialist heritage adviser.
- The Gardens Trust – No adverse comments.
- Health & Safety Executive - Do not advise against the grant of planning permission. Consult pipeline

operator, Cadent Gas re: proximity to high pressure gas main.

- Cadent Gas - No objections. Advisory note re: registering works on Cadent Gas website and reference to guidance.
- Local Residents – One letter of objection; concerns re: crossing of White Hart Lane during the development phase and post construction, pedestrian footfall to and from Sainsbury's and the Beaulieu Park Sports Ground, clarity re: crossing of the new bus link road and speed limits/rising bollards to slow vehicles entering the bus link road.

6. Planning Considerations

Main Issues

- Principle of Development
- Heritage Considerations
- Access Considerations
- Hard & Soft Landscaping
- Ecology & Biodiversity

Principle of Development

Beaulieu Park Railway Station

6.1. The Chelmsford Local Plan promotes major improvements to public transport infrastructure within the city area. The key infrastructure requirements to deliver the objectives of the Spatial Strategy, which include the provision of a new railway station at Beaulieu, are set out in Policy S9 of the Plan. Paragraph 6.62 of the Plan points to the new railway station in north-east Chelmsford, as the means

of improving rail infrastructure from the mid 2020's onwards. Reserved matters were approved for Beaulieu Park Station in two phases; 21/01097/REM and 22/00473/REM refer. Work on the delivery of the new station commenced in 2022.

- 6.2. The station forms the cornerstone of a series of sustainable transport measures for North-East Chelmsford, providing train services for existing and new residents and workers, supporting future business development within the immediate area, as well as existing business activity in Colchester Road and relieving pressure on the existing Chelmsford city centre station.

Beaulieu s106 Agreement

- 6.3. The outline planning permission for the mixed-use residential-led development at Beaulieu (09/01314/EIA refers) established the principle of a two-way bus link road to connect White Hart Lane with Beaulieu Park Railway Station. An application for full planning permission has been submitted for the bus link road, as an area of land, 1.4ha in size, falls outside the outline planning permission red line boundary. Notwithstanding, the outline planning permission is clearly a material planning consideration in the determination of this application.
- 6.4. The s106 Agreement for Beaulieu, specifically obligates the developer to construct and complete a two way bus link road from White Hart Lane to the new Beaulieu Park Railway Station. The link road is to be in place prior to the opening of the railway station. Plan 5, appended to the legal agreement details the route of the bus link. The section of the bus link from White Hart Lane is shown to follow the alignment of the current road with widening of the route to allow for two-way bus movement. The section of the bus link from the junction with The Avenue (the tree lined approach to New Hall School), running parallel with the existing railway line and connecting with the station site, has already been consented under the reserved matters approval for the new station. Once routing into the station site, the bus link road wraps around the built edge of the station, serving a new bus interchange and connecting with the Beaulieu Infrastructure Phase 5 Road (the station access road); which is the subject of a separate planning application; 23/00380/FUL refers.
- 6.5. The bus link road together with the associated pedestrian and cycle paths, which form part of this application, are a key component of the overall sustainable transport strategy for Beaulieu Park Railway Station.

Heritage Considerations

- 6.6. The proposed bus link road lies within an area which historically formed part of the parkland of New Hall, a Grade I listed building, once part of Henry VIII's palace at Beaulieu. The bus link road and Beaulieu Park Railway Station lie either side of a formal tree-lined Avenue, which connected New Hall to the previous London-Colchester Road. The immediate grounds of New Hall and the Avenue are designated as a Grade II Registered Park and Garden. Land to the east and west of the Avenue is in the process of being returned to parkland in accordance with the approved Landscape Design and Management Plan. The Parkland forms one of a series of compensatory measures to offset the acknowledged harm to the Grade I listed building at New Hall and its Grade II Registered Park and Garden arising from the Beaulieu development.
- 6.7. Paragraph 199 of the NPPF advises that when considering the impact of development on the significance of a designated heritage asset, greater weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be). Paragraph 200 states that where a development would result in substantial harm to, or to the loss of, the significance of a designated heritage asset, permission should be refused, unless the harm is necessary to achieve

substantial public benefits, or a number of criteria are met. Paragraph 201 goes on to state that where less than substantial harm is identified, this harm should be weighed against the public benefits of the proposed development.

- 6.8. Extensive discussions and negotiation have taken place between the local planning authority, the Council's Heritage Officer, Historic England, the Highway Authority and the applicant and their advisors prior to the submission of the application. The scheme responds acceptably to the heritage sensitivity of the site, existing below and above ground constraints and the overriding objective to deliver safe, convenient, accessible and well-lit footpaths/cyclepaths to the future railway station. The crossing of the Avenue has been designed to lessen the heritage impact by reducing signage, introducing non-standard surfacing to the footpath/cyclepaths to respect the setting of the Grade I listed building and Grade II Registered Park and Garden, incorporating shorter black painted lighting columns with warm white lighting units as opposed to standard columns with LED lighting units and providing more generous spacing between columns to reduce street clutter. Lengthy negotiation has taken place over the approach to bus gate enforcement, signing and lining again to minimise the heritage impact. Heritage considerations have been to the fore of the scheme design.
- 6.9. The section of the bus link road nearest the tree lined Avenue and bus gate forms the most sensitive element of the scheme, being directly adjacent to New Hall and partly within the Registered Park and Garden. The crossing, signage, lighting and surfacing have been carefully designed to respond to this setting, whilst meeting highway standards for a pedestrian, cycle and bus route together with a main route access into New Hall. The introduction of the bus route and its associated paraphernalia would erode the setting of the Avenue due to its urban character; this harm has been minimised in the design.
- 6.10. The application proposes retention of the existing brick wall and piers including the New Hall School name plaque on the south-eastern side of the entrance. The brick wall and pier on the north-western side of the entrance, which are situated on land within the applicant's ownership would be demolished in order to form a new junction, which would enable two-way bus movement. The wall, pier and stone name plaque would be replaced to maintain a recognisable entrance feature of the same form, design, materials and detail as the existing. The free-standing school sign would also be repositioned behind the boundary wall to reflect the existing arrangement. The road would become adopted highway; for this reason, and as it is intended to facilitate bus access to and from the new railway station, it would not be possible to reinstall the existing lockable gates.
- 6.11. The revised plans make minor changes to the landscaping and boundary treatments and provide additional details for signage. Previous revisions to the drawings have provided further detail in relation to planting adjacent to the drainage pond and details of highway works.
- 6.12. The LDMP sets out a requirement for (i) planting at the southern end of the existing tree-lined Avenue approach to screen railway equipment in views looking south and (ii) for informal trees and hedgerow planting to screen the railway and unsightly boundaries along the narrow corridor adjacent to the railway line, as space allows. The southern side of the bus link road consists of a relatively narrow section of maintained grass; this abuts the operational railway line. A high pressure gas pipe crosses the road close to the existing bend with a large associated easement. A BT fibre cable runs along the southern side of the existing access road. Network Rail stipulations on planting adjacent to a railway boundary further limit the scope for new planting; this is in order that it does not prevent access to a common boundary nor impact upon the railway. Having regard to the existing underground services and the constraints imposed by the adjoining railway line, additional planting in this location is not achievable. The area would therefore be managed as grass. Existing vegetation

is present within Network Rail's own land; this would offer some level of screening between the bus link road and the railway line.

- 6.13. The proposed scheme would result in a low level of less than substantial harm to both the Registered Park and Garden and the setting of New Hall; this is to be weighed against the significant public benefit to be achieved in delivering a sustainable transport route to Beaulieu Park Railway Station, providing pedestrian and cycle access for residents of Beaulieu and Beaulieu Park and a more direct route for residents living in the eastern part of Springfield and Chancellor Park and to businesses and residential properties in Colchester Road. The scheme in this respect accords with Policies S3 and DM13 of the Chelmsford Local Plan and paragraph 201 of the NPPF.
- 6.14. Conditions are recommended to ensure the planting scheme is implemented within the first available planting season and is subject to a management plan, to be agreed and to require agreement to details of signage and its location, line markings and lighting.

Access Considerations

Traffic Movement

- 6.15. The bus link road with its associated pedestrian and cycle access forms a key component of an integrated public transport strategy for Beaulieu Park Railway Station enabling residents of Beaulieu, Beaulieu Park, Springfield and neighbouring residential and employment areas to access the station by non-car modes of travel.
- 6.16. Buses would be operational along the link road between the hours of 05:30 and 23:30. During peak hours 07:00-09:00 and 16:00-1900, buses would run every 10 minutes in both directions equating to a total of 12 buses per hour.
- 6.17. Ongoing ingress along the Avenue by New Hall School traffic would be maintained, alongside use of the widened road as a bus link. The road would become adopted highway and a 30mph speed limit put in place. A bus gate would be provided east of the junction of the bus link road and the Avenue to restrict access to buses and pedal cycles only.

Construction of the Bus Link Road

- 6.18. Lengthy negotiations have taken place regarding the form and design of the bus link road, with a particular focus on signage, lighting and infrastructure necessary to provide bus gate enforcement. Tracking drawings and a Stage 1 Road Safety Audit have been submitted to the Highway Authority and these show that the junction proposals are acceptable. The scheme meets the highway authority's technical and design standards.
- 6.19. The scheme has also been the subject of extensive discussion with the City Councils Parks & Green Spaces team; in this respect its design avoids any operational impact on the Beaulieu Park Recreation Ground, including its parking, access and servicing arrangements.

Highway Land

- 6.20. Essex Highways have undertaken extensive research to establish the position in relation to highway land. A report has been produced and this has been circulated to the local planning authority, the applicant and New Hall School.

- 6.21. The report concludes that the highway extends up to, but does not include, the existing entrance walls and supporting piers; these form a physical barrier to any potential highway rights being established beyond them. The existing wall and piers to be demolished on the north-eastern side of the existing entrance are located on land within the applicant's control and do not form part of the highway. The revised location for the wall and pier is similarly not on highway land.
- 6.22. A trapezium shaped parcel of land in front of the gates is registered to New Hall School. Investigatory work undertaken by Essex Highways concludes that this land also forms part of the highway and therefore is considered to be subject to highway rights. s278 of the Highways Act 1980 can be used to licence a developer, in this case the applicant, to carry out improvement works on a public highway, where they are deemed to be necessary and where planning permission has been granted for a development. Should planning permission be granted for the bus link road, then the applicant would be able to undertake the scheme works in accordance with the approved drawings and discharge of condition details as improvement works on a public highway. The development is considered necessary to facilitate sustainable modes of travel to Beaulieu Park Railway Station.

Bus Gate Enforcement

- 6.23. The bus link would require enforcement to prevent indiscriminate use by private motorists seeking to access the railway station via The Avenue to circumvent a longer journey to the station car park via the Boreham Interchange, the Radial Distributor Road and Beaulieu Infrastructure Phase 5 Road (station access road). An ANPR camera system would be installed, with a camera located opposite The Avenue. A further camera would be placed at the station end of the bus link to similarly prevent private motorists from using the bus link road as a shorter route to access Springfield, Beaulieu Park and Beaulieu. The camera would need to be supplied by the Highway Authority's approved company and the costs met by the applicant.

Pedestrian & Cycle Access

- 6.24. The scheme prioritises pedestrian and cycle access. Having regard to the constraints upon available space, and to avoid any adverse impact on the parking provision and playing pitches associated with the Beaulieu Park Recreation Ground, a 1.2m wide footway is the maximum width of route, which can be provided alongside the bus link road. The path would connect with the at grade signal controlled crossing of White Hart Lane near Oak Lodge Tye. Cyclists would be permitted to use the bus link road and connect with the shared footpath/cyclepath further along the bus link road.
- 6.25. The existing path which wraps around the perimeter of the Beaulieu Park Recreation Ground would be widened to deliver a 3.5m shared footpath/cyclepath. The path would connect with a future primary footpath/cyclepath wrapping around the edge of the Estate Parkland West and would then run parallel to the bus link, crossing The Avenue to connect with the approved footpath/cyclepath being delivered as part of the reserved matters approval for Beaulieu Park Railway Station (22/00473/REM refers). The crossing of the Avenue has been designed to give pedestrians and cyclists a right of way; this aligns with recent changes to the Highway Code with the introduction of the hierarchy of road users. The section of the shared footpath/cyclepath running parallel with Burnell Gate is already lit by street lighting. The remainder of the route would be lit by timber bollard lighting. The path would have a resin bound porous surface in line with other pedestrian/cycle routes leading to Beaulieu Park Railway Station. The path did not form part of the approved Beaulieu Movement Network Strategy but is the result of positive discussion and negotiation between the local planning authority and the applicant delivering a further safe, convenient, accessible and lit from Beaulieu Park

to the railway station. Timber bollards would be required on the roadside edge of the path running parallel with Burnell Gate to prevent private motorists parking on the path.

Access to & Impact upon New Hall School

- 6.26. New Hall School's existing access rights from White Hart Lane across the applicant's land to The Avenue would be unaffected by the bus link. The crossing of The Avenue has been designed to give pedestrians and cyclists a right of way, whilst this would result in school traffic having to stop for a period of time, it must be recognised that pedestrian and cycle flows would not be continuous. Further, any minor delay to journeys needs to be viewed against the clear public benefit to be accrued in delivering a sustainable transport route from several neighbouring residential areas to Beaulieu Park Railway Station. Furthermore, the scheme delivers an upgrade of an existing pedestrian path in existing Beaulieu Park to a fully surfaced, widened and lit footpath/cyclepath, which is in addition to pedestrian/cycle routes already secured through the Beaulieu outline planning permission. Significant investment in, and delivery of, improvements to vehicular, pedestrian and cycle access to New Hall School has also occurred since the development of Beaulieu.
- 6.27. Prior to the construction of the Beaulieu neighbourhood, the school was served by a single access from White Hart Lane (the bus link road) and a single exit via Generals Lane. An unmade footpath extended east-west across the frontage of the school, connecting Generals Lane with existing Beaulieu Park; this was difficult, if not impossible to use in inclement weather. Since the development of Beaulieu, and following the opening of the RDR in June/July this year, the school will be served by three roads which would provide access to the school, (i) from White Hart Lane via the bus link road, (ii) from Generals Lane via New Hall Lane and (iii) to the north from Remembrance Avenue via New Hall Way. Two points of exit already exist, (i) directly onto Generals Lane to the east and (ii) via New Hall Way onto Remembrance Avenue.
- 6.28. The footpath extending east-west across the frontage of New Hall School has been upgraded to a secondary strategic cedec rolled gravel footpath/cyclepath providing an accessible route from Generals Lane to the school and to Beaulieu and Beaulieu Park. The path would in turn connect with a series of primary footpath/cyclepaths, which lead to Beaulieu Park Railway Station and other parts of Beaulieu; these include approved and proposed routes either side of the Estate Parkland and the proposed route around the perimeter of the Beaulieu Park Recreation Ground, which connects with a crossing of The Avenue and a consented footpath/cyclepath direct to the station.
- 6.29. Use of these additional means of vehicular access to the school will inevitably have reduced traffic flow along the route of the bus link road from the position pre-development of Beaulieu. Furthermore, it must reasonably follow that the advent of the new railway station would bring about changes to the way in which students and staff access the school and some bus/car borne trips to the school could reasonably transfer to rail, particularly given the ease with which staff/students could then access the school by pedestrian/cycle. The proposed shared footpath connects with an existing footpath, which runs parallel to the tree lined Avenue and which was implemented by the school. The local planning authority understands that minibuses / buses currently transfer students from the existing Chelmsford City station to the school; with the advent of the new station, this movement could cease thereby further reducing traffic along the bus link road.
- 6.30. The proposed changes to the road junction with White Hart Lane are necessary to allow two-way bus movement, the principle of which was agreed as part of the outline planning permission. The replacement walls and piers maintain a recognisable entrance feature. The provision of a gated entrance, as currently exists would not be possible as the road would become adopted highway, however, the planning permission for the introduction of new security railings and gates at the school;

18/01055/FUL refers, did include consent for a new gate at the main school entrance at the top of the Avenue. On that basis it is not considered that the security of the school would be compromised. Signage at the entrance on White Hart Lane would make clear that the tree lined Avenue would be for New Hall School traffic only.

- 6.31. The scheme prioritises pedestrian and cycle movement, includes measures to enforce use of the bus link road and accords with paragraphs 110 and 112 of the NPPF and Policies S9 and DM24 of the Chelmsford Local Plan.

Hard & Soft Landscaping and Trees

- 6.32. The application has been supported by a Landscape and Visual Impact Assessment, landscaping drawings and an Arboricultural Impact Assessment.
- 6.33. The proposed widening of the road, surfacing and lighting have been assessed against a baseline situation of a surfaced access road; whilst there would be an intensification of use including signage and bus movement, having regard to the existing context where a road already exists, the changes would be unlikely to have significant adverse landscape effects. The proposed development would not impact upon the adjacent Beaulieu Park Recreation Ground, which has been judged as a key attribute in determining the local landscape value.
- 6.34. Existing residential properties in Beaulieu Park to the rear of Honor Link, Wharton Drive and Burnell Gate would have potential views of the proposed bus link road, however, given that the proposed development would be of a similar nature to the existing access, the change would not be of such a magnitude as to result in significant effects. New tree and hedge planting is proposed between the bus link road and the Beaulieu Park Recreation Ground, which in time would provide a good level of screening. Users of the Estate Parkland would experience a small change visually, and elements of the attenuation basins would be visible, but their appearance would be softened by the proposed landscaping.
- 6.35. The approach to landscaping and boundary treatments has been the subject of discussion with the Parks & Green Spaces team due to the impact upon the boundary with the car park serving the Beaulieu Park Recreation Ground and the introduction of planting alongside the playing fields. A substantial section of the existing hedge to the north-west of the existing access fronting White Hart Lane is now proposed to be retained. Planting has been included to the northern side of the bus link adjacent to the recreation ground to an acceptable level whilst not unduly enclosing the footpath/cyclepath.
- 6.36. Additional planting on the southern side of the bus link road is not achievable due to existing underground services and the constraints imposed by the adjoining railway line as set out above; heritage considerations.
- 6.37. Six Category B trees, four Category C trees, one Category C Hedge and parts of a Category C tree, hedge and two groups of trees would need to be removed to enable the development. A further two groups of Category C trees and one Category C hedge need to be pruned and one Category A tree would experience root protection disturbance. Works to trees would be carried out in accordance with the submitted Arboricultural Method Statement. New tree planting and landscaping would mitigate the loss of existing trees and hedges. No objections are raised subject to a condition requiring compliance with the tree report and method statement. The scheme accords with Policies S4, DM17 and DM24 of the Chelmsford Local Plan.

Ecology & Biodiversity

- 6.38. The submitted Pre-Construction Ecological Survey recorded habitats of negligible, low or only moderate value. Two hedgerows planted as part of the landscaping for Beaulieu Park provide potential nesting bird habitat but are not regarded as 'important hedges' in the context of the Hedgerow Regulations. The site extends to the southern margin of an existing attenuation basin, created in early 2000 as a balancing feature for Beaulieu Park, this was found to contain a dense population of fish but no protected species. No likely bat roosting habitat was recorded within the site. The hedges, the New Hall School tree lined Avenue, the rail corridor and the lake were all identified as contributing to the foraging and commuting resources for local bat populations.
- 6.39. The majority of the hedgerows on site would be retained and new hedgerow planting introduced. Bat populations are to be monitored during the operational programme to identify any impacts and mitigate those; subject to a condition requiring adherence to the ecological report, there should be no resultant harm to protected wildlife and no permanent loss of significant habitats. The scheme accords with Paragraph 180 of the NPPF and Policies S4 and DM16 of the Chelmsford Local Plan.

Other Matters

Archaeology

- 6.40. The site has been the subject of archaeological investigation, in the form of fieldwalking and trial trenching in conjunction with the wider Beaulieu development. The County Archaeological Team has advised that this work has uncovered only limited archaeological remains; on that basis there is no requirement for any further work to be undertaken. The scheme accords with Policies S3 and DM15 of the Chelmsford Local Plan.

Noise & Air Quality

- 6.41. Buses would be operational between the hours of 05:30 and 23:30. During peak hours 07:00-09:00 and 16:00-19:00, buses would run every 10 minutes in both directions equating to a total of 12 buses per hour. The Public Health & Protection Service raised initial concerns in relation to noise and air quality, as set out in their response at Appendix 1. The updated Noise Technical Note sets out the results of the noise survey conducted on site, having regard to the expected number of bus vehicle movements and the likely impact on the Gate Lodge, the closest affected property, and existing residential properties in Wharton Drive. The report concludes that noise levels from bus movements would be indistinguishable from the existing noise climate where the dominant noise sources within the area are the A138 and the A130 White Hart Lane.
- 6.42. The bus link road connects with White Hart Lane; this road would be downgraded in 2023 following the opening of the Radial Distributor Road, with consequential reductions in traffic flow along White Hart Lane. Further traffic reductions to the local highway network are also expected to take place when the North-East Bypass opens. The northern access to New Hall School (New Hall Way) has removed traffic from the route of the bus link road. A new bus service is proposed as part of the approved Beaulieu Bus Strategy to service the station; all other bus services are extensions and diversions of existing services. The bus link road forms part of a wider strategy, that promotes better use of public transport and reduces traffic flow and congestion on White Hart Lane. On this basis, the bus link road, forms part of a strategy which would have a positive impact on air quality. The Public Health & Protection Service are satisfied that the updated noise technical note and summary of the

transport strategy and its impact on traffic levels within the surrounding area has addressed their initial concerns. The scheme accords with Policy DM29 of the Chelmsford Local Plan.

Surface Water Drainage

6.43. The application has been supported by a Flood Risk Assessment and Drainage Strategy. The scheme provides for a positive drainage strategy in the form of an unlined swale and attenuation basin located within the area of Estate Parkland between the existing drainage basin and The Avenue. Additional planting has been introduced around the margins of the proposed basin and swale in response to officer concerns to help create a naturalistic appearance reflecting a similar approach to the existing attenuation basin located alongside within the Estate Parkland. No objections have been raised by the County SuD's Team subject to conditions requiring approval of a detailed surface water drainage scheme and maintenance arrangements. The scheme accords with Policies S2 and DM18 of the Chelmsford Local Plan.

High Pressure Gas Pipeline

6.44. A high pressure gas pipeline crosses over the eastern part of the site close to the point where the bus link road connects with the future Beaulieu Park Railway Station and bus gate. The local planning authority has fulfilled its statutory requirement in inputting details of the application into the Health & Safety Executive's Planning Advice Site, which has generated the response below – see Appendix 1. The HSE advised the local planning authority to consult with Cadent Gas, which it has done; they have raised no objections to the application but have requested an informative be attached to the decision notice to ensure their requirements are adhered to.

Representations

6.45. A representation has been received from a local resident raising concerns regarding arrangements for pedestrian and cycle access, the ability to use the existing controlled at grade crossing on White Hart Lane close to the bus link road entrance near Oak Lodge Tye both during and post completion of the works and the intended speed limit for the road.

6.46. The controlled at grade crossing on White Hart Lane near Oak Lodge Tye would remain; it is not envisaged that it would prove necessary to close the crossing during the period of construction works, but if it were, an alternative temporary crossing would be provided.

6.47. A 1.2m wide footway is proposed to run alongside the initial section of the bus link road, extending from the existing footpath on White Hart Lane which connects with the existing controlled at grade crossing. The footway would run parallel to Beaulieu Park Recreation Ground, up to the point where it would meet with the proposed shared primary footpath/cyclepath at the edge of the playing fields. The footpath and bus link road would be lit.

6.48. The scheme incorporates a crossing over The Avenue, which has been designed to give pedestrians and cyclists a right of way. The crossing would connect with the approved footpath/cyclepath being delivered as part of the reserved matters approval for Beaulieu Park Railway Station.

6.49. The existing road has speed limit signs along its length encouraging motorists to reduce their speed as they approach the gated and barrier controlled access to New Hall School. The road is currently not public highway and so the speed limit is not enforceable. The proposed bus link road through the bus gate would become adopted highway. The speed limit will be 30mph by virtue of street lighting; this can be enforced. A speed limit of 20mph would require significant traffic calming and associated

signage, which is a heritage concern. The scheme signage and lighting have been carefully designed to respond appropriately to the setting of the Grade I listed building at New Hall and its Grade II Registered Park and Garden. The introduction of rising bollards on a public highway would not be acceptable to the Highway Authority. The bus link road up until the bus gate would be used by the general public as a route to access New Hall School.

7. s106 Agreement & Community Infrastructure Levy (CIL)

- 7.1 The application is not CIL liable.
- 7.2 The proposed 3.5m shared footpath/cyclepath to be provided around the perimeter of Beaulieu Park Sports Ground and connecting with the shared footpath/cyclepath extending around the western and southern edge of Estate Parkland West is to be provided with a resin bound gravel surface and bespoke timber bollard lighting. The works to construct the path and install the lighting would be undertaken by the applicant. The bollard lighting units would be provided by the City Council and are identical to those already in place at Oaklands Park and Hylands Park. The resin bound gravel surface is not a surface material which Essex County Council as highway authority are willing to adopt. The City Council has agreed to take a commuted sum for the cost of maintaining the resin bound gravel surfaced footpath/cyclepath route and the bespoke timber bollard lighting units.
- 7.3 The City Council has also agreed to maintain the grass verge extending along the southern side of the bus link road on behalf of the County Council. The grass verge would form part of the adoptable highway.
- 7.4 The commuted sums would be secured via a s106 Agreement; this is in the process of being prepared.

8. Conclusion

- 8.1 The application brings forward the requirement for a two-way bus link road from White Hart Lane to Beaulieu Park Railway Station as set out within the s106 Agreement forming part of the outline planning permission for Beaulieu; 09/01314/EIA refers.
- 8.2 The bus link road together with the associated footway and shared footpath/cyclepath have been the subject of extensive pre-application discussion and negotiation between the local planning authority, the Council's Heritage Officer, Historic England, the Highway Authority and the applicant and their advisors prior to the submission of the application. The scheme responds acceptably to the heritage sensitivity of the site, existing below and above ground constraints and the overriding objective to deliver a bus route and safe, convenient, accessible and well-lit footpaths/cyclepaths to the future railway station. The proposed scheme would result in a low level of less than substantial harm to both the Registered Park and Garden and the setting of New Hall; this is to be weighed against the significant public benefit to be achieved in delivering a sustainable transport route to Beaulieu Park Railway Station.
- 8.3 The bus link road together with the associated pedestrian and cycle paths, which form part of this application, are a key component of the overall sustainable transport strategy for Beaulieu Park Railway Station.
- 8.4 The scheme accords with the policies of the development plan and approval is recommended subject to the completion of a s106 Agreement to secure a commuted sum towards future maintenance of the shared footpath/cyclepath and associated timber bollard lighting and maintenance of the grass verge on the southern side of the bus link road by the City Council

RECOMMENDATION

Subject to an agreement, as indicated in the report presented to the Committee, being entered into by 20th September 2023 pursuant to the Town and Country Planning Act 1990, the Director of Sustainable Communities be authorised to grant the application subject to the following conditions:-

Condition 1 Time Limit

The development hereby permitted shall begin no later than 3 years from the date of this decision.

Reason:

In order to comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

Condition 2 Compliance with Plans & Conditions

The development hereby permitted shall be carried out in accordance with the approved plans and conditions listed on this decision notice.

Reason:

In order to achieve satisfactory development of the site

Condition 3 Commencement of Construction Works

No construction works shall commence until Phase 3 of the RDR, reserved matters approvals 20/00190/REM and 20/00207/REM refer, has been completed and the bridge and road are open to the public.

Reason:

To ensure adequate access is made available at all times to New Hall School.

Condition 4 Timing of Delivery of the Bus Link Road

The bus link shall be constructed and operational prior to the opening of the adjacent Beaulieu Park Railway Station.

Reason:

To ensure the adjacent Beaulieu Park Railway Station has suitable sustainable travel links and direct access for passenger transport to the urban area of Springfield.

Condition 5 Pre-Construction Ecological Survey

The development hereby permitted shall be constructed wholly in accordance with the licensing, mitigation and enhancement measures and requirements contained within the Pre-Construction Ecological Survey for the Beaulieu Bus Link Application Area prepared by Richard Graves Associates dated July 2022; any variation to the measures and requirements shall be agreed in writing with the local planning authority.

Reason:

In the interests of wildlife habitat protection in accordance with Policies S4 and DM16 of the Chelmsford Local Plan.

Condition 6 Compliance with the Tree Report

The development hereby permitted shall be constructed wholly in accordance with the tree protection, construction methods and recommendations contained within the Arboricultural Impact Assessment (APIII): Bus Link - Beaulieu Park prepared by Hallwood Associates dated 11th August 2022 Ref: HWA10724_APIII Version 2.0.

Reason:

To safeguard the existing trees in accordance with Policies S4 and DM17 of the Chelmsford Local Plan.

Condition 7 Bus Link Road Adoption Details

The bus link to be provided shall form an adopted highway and be constructed to an adoptable standard; the precise design to be agreed in consultation with the local planning authority and the highway authority and delivered by the developer under the Highways Act 1980. The bus link shall include features necessary to enable bus gate enforcement to prevent any vehicles other than buses and pedal cycles from using the bus link; this shall include the equipment required for the enforcement to be supplied and installed by the company approved by Essex County Council (the full cost to be met by the developer), signing and lining works and necessary Traffic Regulation Orders.

Reason:

To ensure that the bus link is designed to an acceptable and adoptable standard and that the required infrastructure is in place to allow for the installation of enforcement of the bus link, at a time to be agreed with the highway authority, so that it can function as a controlled bus route to serve Beaulieu Park Railway Station.

Condition 8 Surface Materials and Bollards

Notwithstanding the details shown on the approved landscaping plans; Drawing No's DR-0002 Rev S4-P8 - Landscape Layout, DR-0003 Rev S4-P8 - Detailed Layout 1 of 3, DR-0004 Rev S4-P8 - Detailed Layout 2 of 3, DR-0005 Rev S4-P8 - Detailed Layout 3 of 3 and prior to the commencement of development, details of the external surface materials to be used in the construction of the upgraded 3.5m wide footpath/cyclepath extending around the perimeter of the Beaulieu Park Recreation Ground and connecting with the crossing of The Avenue together with details of the location of timber bollards to prevent vehicle parking on the section of the footpath/cyclepath fronting Burnell Gate shall be submitted to and approved in writing by the local planning authority.

Reason:

To ensure that the proposed surface material is visually satisfactory having regard to the setting of the Grade I listed New Hall and the Grade II Registered Park & Garden, able to be maintained in the long term and suitable for drainage purposes and to prevent vehicle parking obstructing or narrowing the available width of the footpath/cyclepath in accordance with Policies S3, S9, DM13 and DM24 of the Chelmsford Local Plan

Condition 9 Implementation of Planting

The planting as shown on the approved landscaping plans; Drawing No's DR-0002 Rev S4-P8 - Landscape Layout, DR-0003 Rev S4-P8 - Detailed Layout 1 of 3, DR-0004 Rev S4-P8 - Detailed Layout 2 of 3, DR-0005 Rev S4-P8 - Detailed Layout 3 of 3 shall be implemented in the first available planting season in accordance with the approved plans; subject to such minor variations to the timing of delivery as may be agreed in writing by the local planning authority.

Reason:

To ensure early delivery of the planting for the purposes of establishing its maturity at an earlier stage in the development to mitigate the impacts of the bus link road and to compensate for the erosion of the setting of the Grade I listed New Hall and its Grade II Registered Park Garden in accordance with Policies S3, S4, DM13, DM16 and DM17 of the Chelmsford Local Plan.

Condition 10 Landscape Management Plan

Prior to the first beneficial use of the bus link road, a landscape management plan detailing the management responsibilities and a programme of maintenance specifications for the areas of planting for a minimum period of five years shall be submitted to and approved in writing by the local planning authority.

Reason:

To ensure satisfactory arrangements are in place to allow for the proper management and maintenance of all the landscaped areas in accordance with Policies S3, S4, DM13, DM16 and DM17 of the Chelmsford Local Plan.

Condition 11 SuD's Scheme

Prior to the commencement of development, a detailed surface water drainage scheme for the site, based on sustainable drainage principles and an assessment of the hydrological and hydro geological context of the development, shall be submitted to and certified as technically acceptable in writing by the SUDs approval body or other suitably qualified person(s). The certificate shall thereafter be submitted by the developer to the local planning authority as part of the developer's application to discharge the condition. No development shall commence until the detailed scheme has been approved in writing by the local planning authority. The approved scheme shall subsequently be implemented prior to first beneficial use of the bus link road and should include but not be limited to:

- Limiting discharge rates to 1.88l/s for all storm events up to, and including, the 1 in 100 year rate plus 40% allowance for climate change,
- Provide sufficient storage to ensure no off-site flooding as a result of the development during all storm events up to, and including the 1 in 100 year plus 40% climate change event,
- Demonstrate that all storage features can half empty within 24 hours for the 1 in 30 plus 40% climate change critical storm event,
- Final modelling and calculations for all areas of the drainage system,
- The appropriate level of treatment for all run-off leaving the site, in line with the Simple Index Approach in Chapter 26 of the CIRIA SuD's Manual C753,
- Detailed engineering drawings of each component of the drainage scheme,
- A final drainage plan, which details exceedance and conveyance routes, finished levels and ground levels and the location and sizing of any drainage features,
- A written report summarising the final strategy and highlighting any minor changes to the approved strategy.

Reason:

To ensure that the development is constructed sustainably in accordance with Policies S2 and Policy DM18 of the Chelmsford Local Plan.

Condition 12 SuD's Scheme Management

Prior to the first beneficial use of the bus link road, details of how the scheme shall be maintained and managed after completion, including the arrangements for adoption by any public body or statutory undertaker, or any other arrangements to secure the operation of the sustainable urban drainage scheme through its lifetime, shall be submitted to and approved in writing by the local planning authority.

The scheme shall thereafter be managed in accordance with the approved details.

Reason:

To ensure that the development is constructed sustainably in accordance with Policies S2 and Policy DM18 of the Chelmsford Local Plan.

Condition 13 Contamination

Prior to the commencement of development, a scheme to assess and deal with any contamination of the site shall be submitted to and approved in writing by the local planning authority. Prior to the first beneficial use of the bus link road, any remediation of the site found necessary shall be carried out, and a validation report to that effect submitted to the local planning authority for written approval. The development shall be carried out in accordance with the approved scheme.

Reason:

To ensure that the proposed development does not cause pollution of controlled waters and that any contamination of the site is identified and remediation measures appropriately undertaken to secure full mitigation in the interests of environmental and public safety in accordance with Policy DM30 of the Chelmsford Local Plan.

Condition 14 Lighting Scheme

Prior to the commencement of development details of the location and design of all lighting columns and lighting bollards along the bus link road/footway and footpath/cyclepath shall be submitted to and approved in writing by the local planning authority.

Reason:

To ensure that the proposed lighting is visually satisfactory and to safeguard the setting of the Grade I listed New Hall and its Grade II Registered Park and Garden in accordance with Policy S4 and DM13 of the Chelmsford Local Plan.

Condition 15 Construction Environmental Management Plan

Prior to the commencement of development, a Construction Environmental Management Plan shall be submitted to and approved in writing by the local planning authority. The Plan shall be adhered to throughout the duration of the construction period and shall include details of the:-

- (i) Construction vehicle access and routing,
- (ii) Any temporary traffic management/signage,
- (iii) Wheel and underside chassis cleaning facilities to prevent the deposition of mud or other debris onto the highway network/public areas,
- (iv) Contractor and visitor parking clear of the highway,
- (v) Highway safety considerations,
- (vi) Turning, loading and unloading of plant and materials,
- (vii) Hours of deliveries,
- (viii) Phasing and timing of construction works for both the development of the Beaulieu Bus Link Road hereby permitted and the Beaulieu Phase 5 Infrastructure Road, 23/00380/FUL, and
- (ix) Phasing and timing of construction works to ensure that two points of vehicular access are maintained to New Hall School during the construction works unless otherwise agreed in the Construction Environmental Management Plan.

The development shall be undertaken in accordance with the approved Construction Environmental Management Plan.

Reason:

In the interests of highway safety and to safeguard the amenities of the occupiers of neighbouring residential properties and to ensure adequate access is available at all times to New Hall School in accordance with Policies DM29 of the Chelmsford Local Plan.

Notes to Applicant

- 1 **s106 Agreement**
You are reminded that this permission is also subject to a legal agreement, and that the terms of this agreement must be complied with.
- 2 **Highway Works**
All work within, or affecting existing and future highways, shall be laid out and constructed by prior arrangement with, and to the requirements and satisfaction of, the Highway Authority; details to be agreed before the commencement of works. You are advised to contact the Development Management Team by email at development.management@essexhighways.org.
- 3 **Highway Agreements**
Prior to any works taking place within the public highway or areas to become public highway, the developer shall enter into an appropriate agreement with the Highway Authority to regulate construction works; this will include the submission of detailed engineering drawings for approval and a safety audit.
- 4 **Liability**
The Highway Authority cannot accept any liability for cost associated with a developer's improvement; this includes design check safety audits, site supervision, any commuted sum for maintenance and any potential claims under Part 1 and Part 2 of the Land Compensation Act 1973; to protect the Highway Authority against such compensation claims a cash deposit or bond may be required.
- 5 **Traffic Regulation Orders**
You are advised that there is an expectation that the developer will pay for the necessary Traffic Regulation Orders together with the provision of the associated signing and lining to prevent parking within the vicinity of the site, including nearby residential areas and to enable enforcement of the bus gate.
- 6 **Equipment and Structures in the Highway**
Any signal equipment, structures and non-standard materials proposed within the existing extent of the public highway, or areas to be offered to the Highway Authority for adoption as public highway, will require a contribution (commuted sum) to cover the cost of future maintenance for a period of 15 years following construction.

Positive and Proactive Statement

The Local Planning Authority provided advice to the applicant before the application was submitted and also suggested amendments to the proposal during the life of the application. The Local Planning Authority has assessed the proposal against all material considerations including planning policies and any comments that may have been received. The planning application has been approved in accordance with the objectives of the National Planning Policy Framework to promote the delivery of sustainable development and to approach decision taking in a positive way.

Appendix 1 – Consultations

Boreham Parish Council

Comments
07.10.22 No objections.

Springfield Parish Council

Comments
18.10.22 & 14.12.22 No objections.

CCC Parks & Open Spaces

Comments
01.06.23 Parks & Open Spaces are broadly happy with the layout and details provided. The footpath/cyclepath which runs parallel to Burnell Gate appears to show the lighting bollards on the park side meaning that Burnell Gate remains unprotected from people parking on the footpath as is currently the case. Road edge bollards should be provided as a defensive means of preventing parking in the area. Street lighting is already in place along Burnell Gate, therefore the need for low level lighting bollards within this stretch is questioned given the high level street lighting opposite.
14.11.22 1. The V mesh fencing running along the southern side of the sports ground car park is intended to be planted up with a double row of native hedging. The approach will ensure that the hedge will entirely engulf the v mesh fence as the hedge establishes and will be maintained as such in the future; this will ensure that over time the v mesh fence is no longer visible whilst providing a suitable barrier in the meantime. The landscape feature is considered satisfactory as a suitable park boundary, effectively screening both aspects; the car park and the bus lane. The established future mean hedge height is anticipated to be maintained at 1.4m. 2. The 3.5m footpath/cyclepath along the northern and north-eastern boundary of the sports ground does not show a bollard solution to mitigate the current parking on the loose surfaced path; there are also no lighting proposals included. 3. The number of individual specimen trees proposed for the southern boundary on the soil bund along the pitches and around the access points where the 3.5m paths join the bus route and run parallel should be increased; this will help provide further screening and over time stop errant sport balls finding their way onto the bus link route and the footpath.

CCC Property Services

Comments
21.11.22 No comments.

CCC Public Health & Protection Service

Comments
07.12.22 The submitted letter satisfies the Public Health & Protection Service queries regarding air quality. No further comments.
21.10.22 The application is unclear as to how many buses or other vehicles will be using the road link or White Hart Lane. The Noise Technical Note explains that buses will be operational between 05:30 and 23:30 at up to 12 times per hour; this suggests that there will be more than 100 HDV movements AADT, which is indicative criteria for requiring an air quality assessment. The applicant needs to a) provide traffic details for the road link and surrounding area and b) provide an air quality impact assessment if the traffic impact of the proposal exceeds the criteria as set out in the IAQM Land Use Planning and Development technical guidance.

Essex County Council Highways

Comments
24.05.23 The application seeks to provide a sustainable travel link (bus, cycle and pedestrian) between White Hart Lane, the existing urban area and the new Beaulieu Park Railway Station. The requirement for a link is set out in the 2009 outline planning permission for Greater Beaulieu Park under 09/01314/EIA. The existing single track, which runs parallel with the railway line and provides an access to New Hall School would be widened to two lanes, the junction with White Hart Lane altered and a foot and cycle route provided adjacent to the carriageway with connections provided to the north. Extensive discussion has been undertaken prior to the application's submission and during the consultation period, between the local planning authority, the applicant, and the Highway Authority. Since the application is in close proximity to New Hall (Grade I listed) and its Registered Park and Garden (Grade II listed), significant care has had to be taken that the development will not negatively affect the heritage of the listed buildings and land. Consequently, there has been lengthy negotiations relating to signage and highway infrastructure to be provided to ensure they are sensitive to the site's setting. The crossing arrangement of the Avenue has been designed to lessen the heritage impact by reducing signage. Non-standard surfacing is proposed on the footway/cycleway to minimise the heritage impact; this will be maintained by CCC going forward.

The crossing on the Avenue has been designed to prioritise pedestrians and cyclists over vehicles; this aligns with recent changes to the Highway Code with the introduction of the hierarchy of road users. Whilst this priority measures will result in vehicles giving way to pedestrians and cyclists, New Hall School has two other access points so visitors can choose to access via other points or utilise sustainable travel modes.

Bus Gate Enforcement

The bus link will require enforcement to prevent indiscriminate use, in the form of an ANPR camera system with a camera located opposite The Avenue and one at the station end of the bus link, together with associated signage. The enforcement cameras must be supplied by the company that provide CCC enforcement systems. The cost for enforcement of the bus gate is to be met by the developer, Countryside Zest.

Tracking and a Stage 1 Road Safety Audit for the White Hart Lane junction and The Avenue junction has been submitted to the Highway Authority and these show that the junction proposals are acceptable.

Submitted Drawing No. 1002 Rev P4 shows the extent of highway adoption, however, this will require amendment as part of the s278 process to include the area where the ANPR camera will be located as this has yet to be determined.

From a highway and transportation perspective, the impact of the proposal is acceptable to the Highway Authority subject to the following mitigation and conditions:

1. No construction works to commence until Phase 3 of the RDR has been completed and open to the public.
Reason: To ensure adequate access is available at all times to New Hall School.
2. Construction works for the bus link and Phase 5 Infrastructure Road (23/000380/FUL) not to occur concurrently. Reason: To ensure adequate access is made available at all times to New Hall School.
3. The bus link to be constructed and operational prior to the opening of the adjacent railway station.
Reason: To ensure the railway station has suitable sustainable travel links and direct access for passenger transport to the urban area of Springfield.
4. The bus link to be provided, shall form an adopted highway and be constructed to an adoptable standard; the precise design to be agreed in consultation with the highway authority and delivered by the developer under s278 of the Highways Act 1980, and shall include features necessary to enable bus gate enforcement to prevent any vehicles other than buses and pedal cycles from using the bus link, including the equipment required for the enforcement to be supplied and installed by the company approved by ECC (the full cost of which will be covered by the developer), signing and lining and necessary traffic regulation orders. Reason: To ensure that the bus link is designed to an acceptable and adoptable standard and to ensure that the required infrastructure is in place to allow for the installation of enforcement of the bus link, at a time to be agreed with the highway authority, so that it can function as a controlled bus route to serve the railway station.
5. Prior to commencement a Construction Management Plan shall be produced and adhered to throughout the construction period. The Statement shall provide for:
 - (i) construction vehicle access and routing,
 - (ii) any temporary traffic management/signage,
 - (iii) wheel and underside chassis cleaning facilities to prevent the deposition of mud or other debris onto the highway network/public areas,
 - (iv) contractor and visitor parking clear of the highway,
 - (v) highway safety considerations,
 - (vi) turning, loading and unloading of plant and materials, and
 - (vii) hours of deliveries.

Details shall be submitted to and approved in writing by the local planning authority. Reason: In the interests of highway safety.

Informatives

1. All work within, or affecting the highway is to be laid out and constructed by prior arrangement with, and to the requirements and satisfaction of, the Highway Authority, details to be agreed before the commencement of works.
2. The applicants to be advised to contact the Development Management Team by email at development.management@essexhighways.org
3. Prior to any works taking place within the public highway or areas to become public highway, the developer shall enter into an appropriate agreement with the highway authority to regulate construction works. The agreement will include the submission of detailed engineering drawings for approval and a safety audit.
4. The highway authority cannot accept any liability for costs associated with a developer's improvement; this includes design check safety audits, site supervision, commuted sums for maintenance and any potential claims under Part 1 and Part 2 of the Land Compensation Act 1973 in order to protect the highway authority against such compensation claims a cash deposit or bond may be required.
5. The developer to pay for the necessary Traffic Regulation Orders together with the provision of the associated signing and lining to prevent parking in the vicinity of the site (including nearby residential areas) and to enable enforcement of the bus gate.
6. Any signal equipment, structures and non-standard materials proposed within the existing extent of the public highway, will require a contribution (commuted sum) to cover the cost of future maintenance for a period of 15 years following construction.

Essex County Council SuD's Team

Comments

26.10.22

No objections subject to conditions to secure the following:

1. No works except demolition to take place until a detailed surface water drainage scheme for the site, based on sustainable drainage principles and an assessment of the hydrological and hydrogeological context of the development has been submitted to and approved in writing by the local planning authority. The scheme to include, but not be limited to:
 - Limiting discharge rates to 1.88l/s for all storm events up to, and including the 1 in 100 year rate plus 40% allowance for climate change,
 - Provision of sufficient storage to ensure no off-site flooding as a result of the development, during all storm events up to, and including, the 1 in 100 year plus 40% climate change event,
 - Demonstrate that all storage features can half empty within 24 hours for the 1 in 30 year plus 40% climate change critical storm event,
 - Final modelling and calculations for all areas of the drainage system,
 - The appropriate level of treatment for all runoff leaving the site, in line with the Simple Index Approach in Chapter 26 of the CIRIA SuD's Manual C753,
 - Detailed engineering drawings of each component of the drainage scheme,
 - A final drainage plan, which details exceedance and conveyance routes, FFL and ground levels and the location and sizing of any drainage features, and

- A written report summarising the final strategy and highlighting any minor changes to the approved strategy.

The scheme to be implemented prior to first use of the bus link road.

2. Prior to first use of the bus link road, a maintenance plan detailing the maintenance arrangements, including who is responsible for different elements of the surface water drainage system and the maintenance activities/frequencies has been submitted to and agreed in writing by the local planning authority.
3. The applicant or any successor in title must maintain yearly logs of maintenance, which should be carried out in accordance with any approved maintenance plan; these must be available for inspection upon a request by the local planning authority.

The applicant is strongly advised to look at the Essex Green Infrastructure Strategy to ensure that the proposals implement multifunctional green/blue features effectively.

Essex County Council Historic Environment Branch

Comments
04.10.22 The site has previously been the subject of archaeological investigation, in the form of fieldwalking and trial trenching, both of which uncovered only limited archaeological remains. No archaeological recommendations.

National Highways

Comments
20.10.22 No objections.

Historic England

Comments
20.10.22 The City Council should seek the views of its specialist conservation and archaeological advisers.

The Gardens Trust

Comments
18.10.22 The response follows liaison with Essex Gardens Trust and its local knowledge informs the joint response. The new bus link would have a minor effect on the Grade II Registered Park & Garden of New Hall, Boreham as it crosses the avenue of the house at its southern end, however a through heritage statement has supported the application, which outlines the harm and flags that as part of the proposal, portions of the land to each side are to be returned to Parkland.

The impact on the historic avenue is acknowledged but the proposed enhancements to the setting and wider/former Parkland are to be welcomed, despite the additional cumulative impact on the Avenue.

Health & Safety Executive

Comments

27.10.22

The Health & Safety Executive (HSE) does not advise on safety grounds, against the grant of planning permission in this case.

The HSE has advised that the proposed development site lies within the consultation distance (CD) of major hazard pipeline. The pipeline operator will need to be contacted as there may be additional constraints on development near the pipeline; 7577_1831 Cadent Gas Limited.

Two reasons are given for this (i) the operator may have a legal interest (easement, wayleave etc) in the vicinity of the pipeline, which may restrict certain developments within a certain proximity of the pipeline and (ii) the standards to which the pipeline is designed and operated, may restrict occupied buildings, or major traffic routes, within a certain proximity of the pipeline; consequently there may be a need for the operator to modify the pipeline, or its operation, if the development proceeds.

Cadent Gas, the pipeline operator, has been consulted – see below.

Cadent Gas

Comments

15.11.22

Cadent Gas has completed its assessment and raises no objection to the proposed scheme but does have high pressure assets in the vicinity of the work area.

Cadent Gas will need to be contacted as the project moves forward to protect its asset.

The applicant should refer to the building protection distance and refer to the 'Specification for Safe Working in the Vicinity of Cadent Assets Ref: CAD/SP/SSW/22 dated August 2021' guidance for working near a high pressure gas pipeline and MP main in the works area.

Should the scheme affect one of Cadent Gas's high pressure pipelines, then it is a statutory requirement that the local planning authority input details of the application into the HSE's Planning Advice Web App; further details – www.hse.gov.uk/landuseplanning/planning-advice-web-app.htm

The HSE may wish to apply more stringent criteria for building proximity after assessment.

Cadent Gas recommends that an informative is appended to the decision notice as follows:

1. The applicant is to be informed that prior to carrying out works, it is to register details of the planned works for review on www.linesearchbeforeudig.co.uk, to ensure requirements are adhered to. Cadent Gas's original holding objection was triggered due to the presence of a High Pressure Major Accident Hazard Pipeline (MAHP) and/or an Intermediate Pressure Pipeline and/or an Above Ground Installation. The minimum building proximity distances for the pipelines and installations have been provided. The building proximity distance is taken from The Institution of Gas Engineers and Managers Publication IGEM/TD/1 Edition 5, which is the standard applicable to steel pipelines and associated installations for high pressure gas transmission and IGEM/TD/3 Edition 5 Steel and PE pipelines for gas distribution.

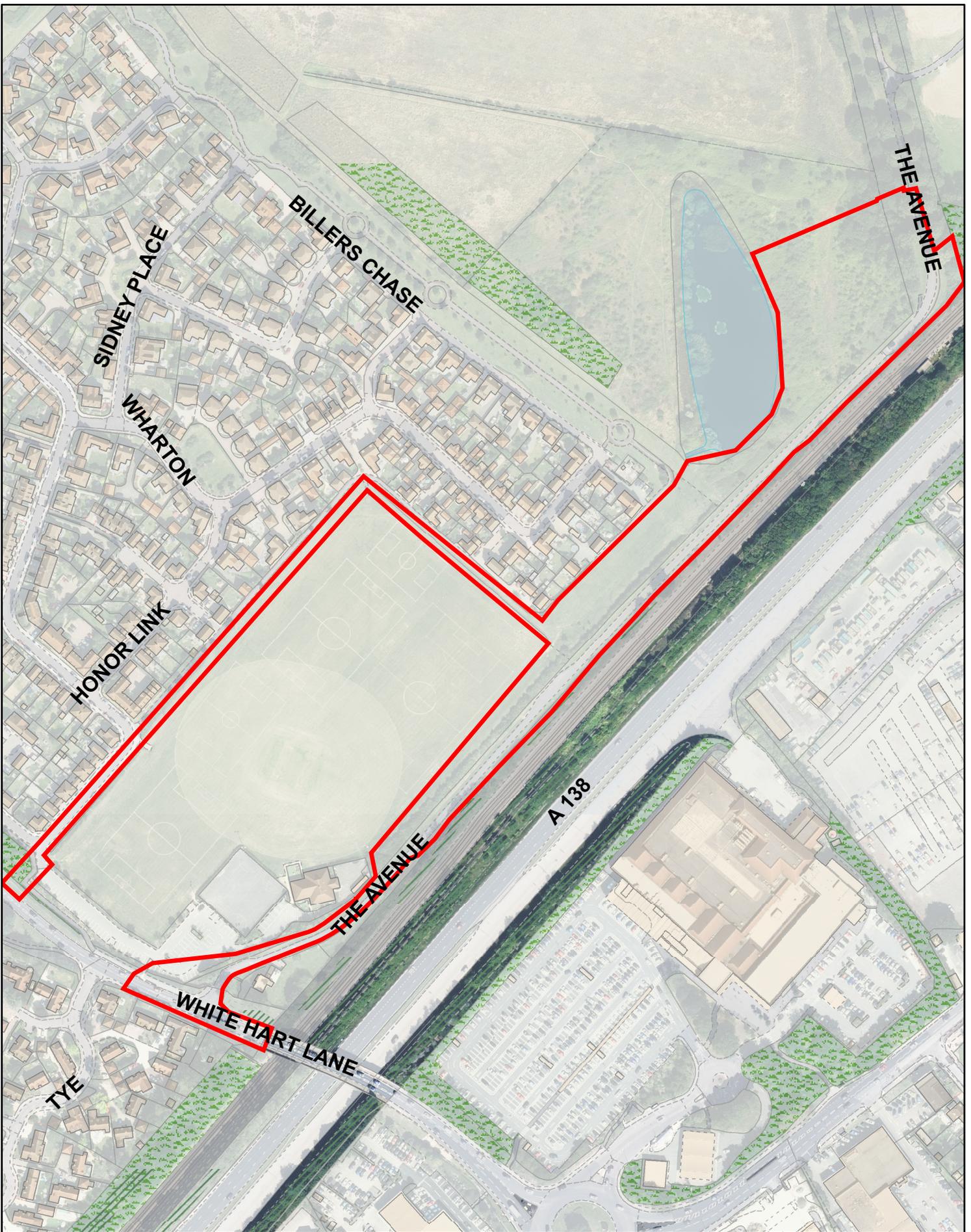
Local Residents

Comments

31.10.22

One letter of representation has been received which raises the following objections:

1. Object if it is not possible to cross White Hart Lane using the existing toucan crossing nearest the railway line and New Hall School during the development phase as well as when the project is completed; it is unclear from the plans if this is possible.
2. Considerable pedestrian footfall to and from Sainsburys and the playing fields.
3. Clarity is required as to how it would be possible to cross the new bus link road on foot; would there be a further toucan crossing as this would be a wide busy junction.
4. A rising bollard system is recommended to slow vehicles and to further increase pedestrian safety and dissuade unauthorised traffic at the White Hart Lane turning. 30mph is too quick. 20mph would be in line with the remainder of the Beaulieu development. Pedestrians using the footpath provided (4 foot wide) would then be safer.



0 30 60 120 Metres

1:2,794



Planning Committee
22/01723/FUL

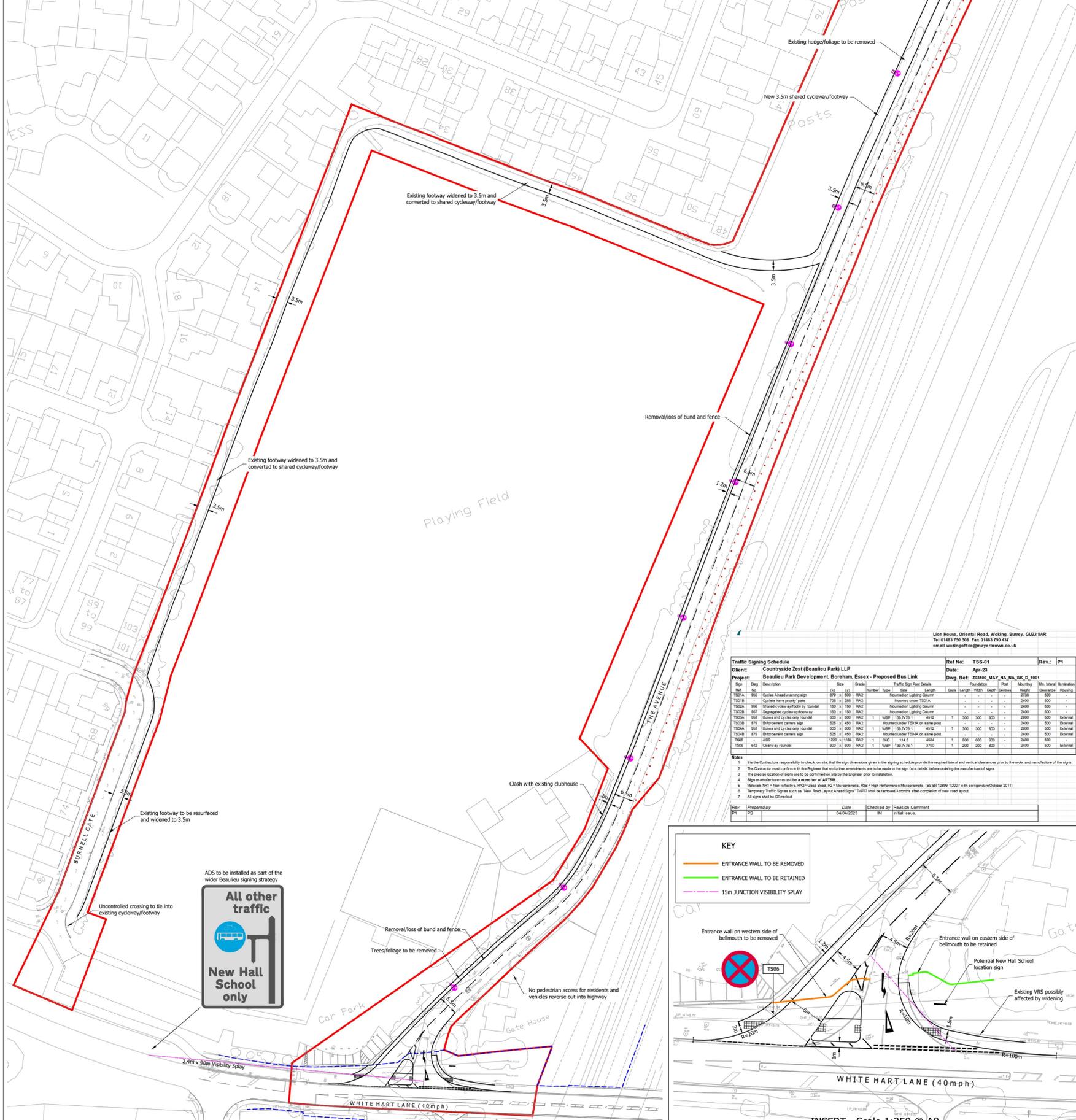
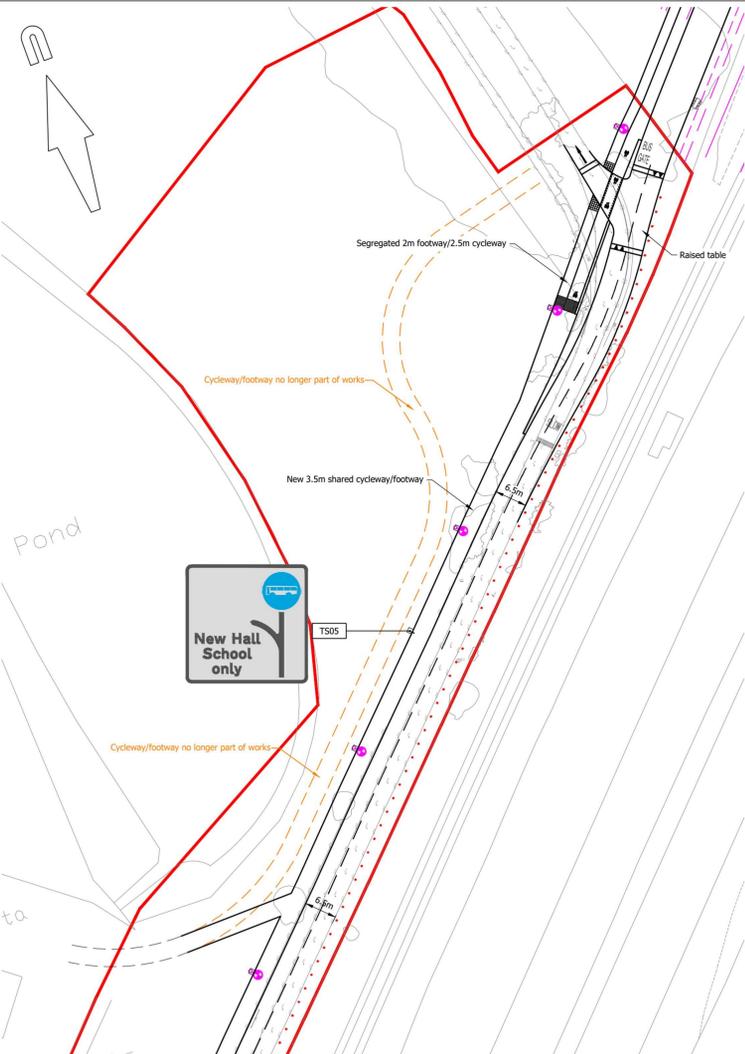
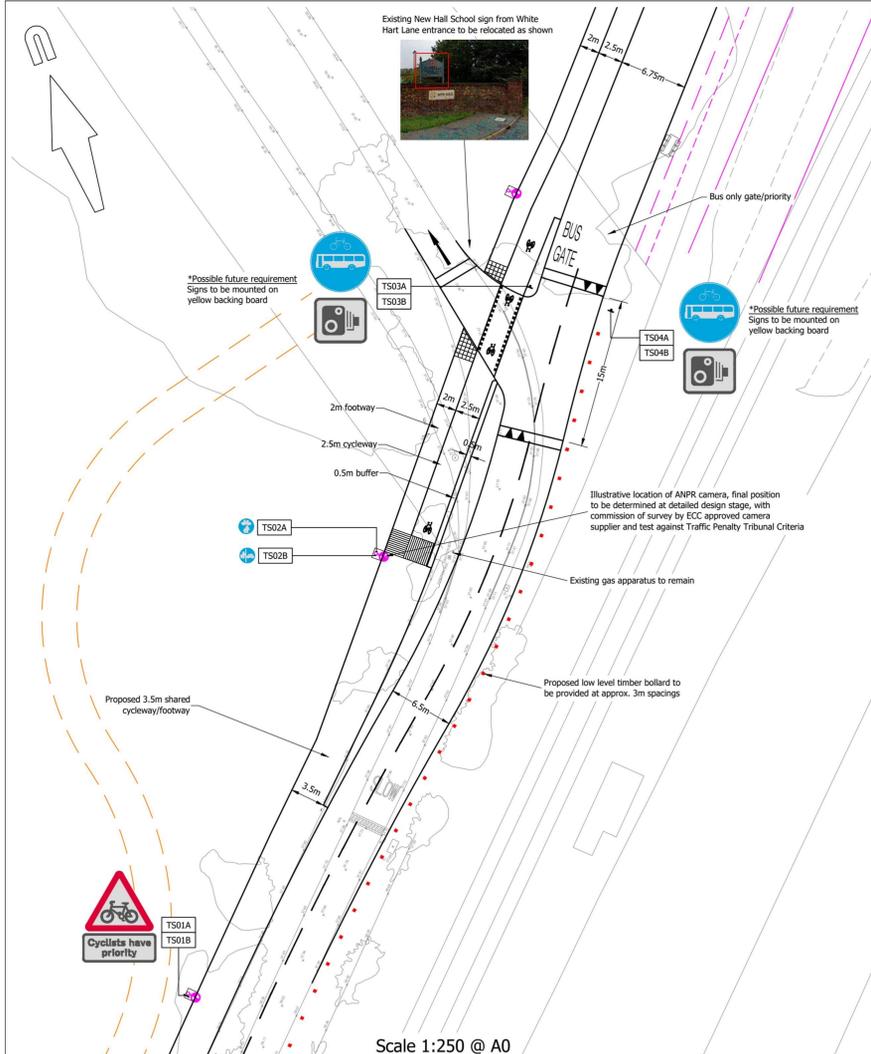
Planning & Development Management
Directorate for Sustainable Communities

PO Box 7544 Civic Centre
Duke Street, Chelmsford, CM1 1XP

Telephone: 01245 606826

Based upon the Ordnance Survey mapping with the permission of the Controller of Her Majesty's Stationary Office. Crown copyright.
Unauthorised reproduction infringes Crown copyright and may lead to prosecution or civil proceedings

Chelmsford City Council Licence Number 100023562 2023.



KEY

- EXISTING KERB LINE
- PROPOSED KERB/EDGING LINE
- PROPOSED TRAFFIC SIGN
- PROPOSED TRAFFIC SIGN REFERENCE
- PROPOSED STREET LIGHTING COLUMN
- PROPOSED TACTILE PAVING
- SEGREGATED FOOTWAY / CYCLEWAY DELINEATION PAVING
- PROPOSED LOW LEVEL TIMBER BOLLARD (EXACT SPEC TO BE CONFIRMED)
- LIMIT OF WORKS BOUNDARY

- NOTES**
- Traffic signing on White Hart Lane undertaken as part of S278 works (to be agreed at detailed design stage).

PRELIMINARY
NOT FOR CONSTRUCTION

© Mayer Brown Limited Copyright. The drawings, information and data recorded in this document ("the information") is the property of Mayer Brown. This document and the information are solely for the use of the authorized recipient and this document may not be copied, stored or reproduced in whole or in part for any purpose other than which it was created by Mayer Brown. Mayer Brown accepts no responsibility for any errors or omissions in this document or the information.

A0 ORIGINAL

Rev.	Description	Date	By
P13	Drawing revised following meeting on 03/04/23. (PB)	04/04/2023	IM
P12	ANPR camera note amended. (PB)	20/03/2023	IM
P11	Shared cycleway/footway north of New Hall School access replaced with segregated facility. (PB)	15/12/2022	IM
P10	Addition of Visibility Splay. (PA)	09/11/2022	IM
P9	Wall shown to be retained on eastern side of bellmouth. (PB)	02/08/2022	IM
P8	Red line amended to accommodate drainage. (PB)	13/07/2022	IM
P7	Footway/cycleway spur amended to avoid trees. (PB)	06/07/2022	IM
P6	New Hall School sign added, ANPR camera note updated. (PB)	04/04/2022	IM
P5	Amended following comments from ECC and further detail added. (PB)	24/03/2022	IM
P4	Red line amended adjusted. (PB)	02/03/2022	IM
P3	Option 2 crossing adjusted. (PB)	23/02/2022	IM
P2	Zebra/parallel crossing, lighting, red line boundary and level added. (PB)	06/12/2021	IM
P1	Initial issue (PB)	18/08/2021	IM

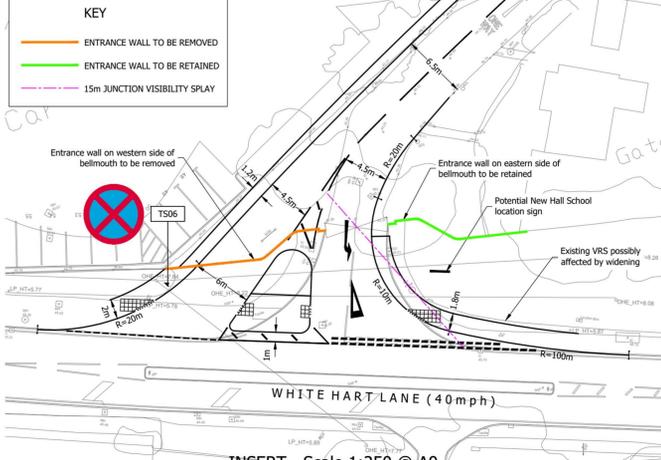
Traffic Signing Schedule

Ref No.	Code	Description	Size	Grade	Number	Type	Sign	Length	Chgs	Width	Depth	Centre	Mounting	Min. lateral clearance	Burrows
TS01A	950	Cyclists have priority sign	278	A0	1	RA2	Mounted on Lighting Column	-	-	-	-	-	2400	800	-
TS01B	950	Cyclists have priority plate	738	A0	1	RA2	Mounted under TS01A	-	-	-	-	-	2400	800	-
TS02A	968	Segregated cycleway/footway	150	A0	1	RA2	Mounted on Lighting Column	-	-	-	-	-	2400	800	-
TS02B	967	Segregated cycleway/footway	150	A0	1	RA2	Mounted on Lighting Column	-	-	-	-	-	2400	800	-
TS02C	967	Bus and cycle only round	600	A0	1	RA2	Mounted under TS02A on same post	412	1	300	300	800	2400	800	Skewed
TS02D	970	Enforcement cycleway sign	625	A0	1	RA2	Mounted under TS02A on same post	412	1	300	300	800	2400	800	Skewed
TS02A	965	Bus and cycle only round	600	A0	1	RA2	Mounted under TS02A on same post	412	1	300	300	800	2400	800	Skewed
TS02B	970	Enforcement cycleway sign	625	A0	1	RA2	Mounted under TS02A on same post	412	1	300	300	800	2400	800	Skewed
TS02C	965	ADS	1200	A0	1	RA2	114.3	4884	1	600	600	800	2400	800	Skewed
TS02D	942	Clearway round	600	A0	1	RA2	138.76	5700	1	200	200	800	2400	800	Skewed

Notes

- It is the Contractor's responsibility to check, on site, that the sign dimensions given in the signing schedule provide the required lateral and vertical clearances prior to the order and manufacture of the signs.
- The Contractor must confirm to the Engineer that no further amendments are to be made to the sign face details before ordering the manufacture of signs.
- The precise location of signs are to be confirmed on site by the Engineer prior to installation.
- Signs as manufacturer must be a member of ARTSM.
- Materials VRS in high reflective 942-Glass Bead, R2-Microspheres, R20-High Performance Materials. (BS EN 12899-1:2007 in conjunction October 2011)
- Temporary Traffic Signs such as "New Road Layout Ahead Sign" TAPP shall be removed 3 months after completion of new road layout.
- All signs shall be CE marked.

Rev.	Prepared by	Date	Checked by	Reason/Comment
P1	IM	20/04/2023	IM	Initial Issue



mB
mayer brown

Mayer Brown Limited
Lion House, Oriental Road, Woking, Surrey, GU22 8AR
Telephone 01483 750 508 Fax 01483 750 437
email: wokingoffice@mayerbrown.co.uk www.mayerbrown.co.uk

client: COUNTRYSIDE ZEST (BEAULIEU PARK) LLP

project: BEAULIEU PARK DEVELOPMENT BOREHAM, ESSEX

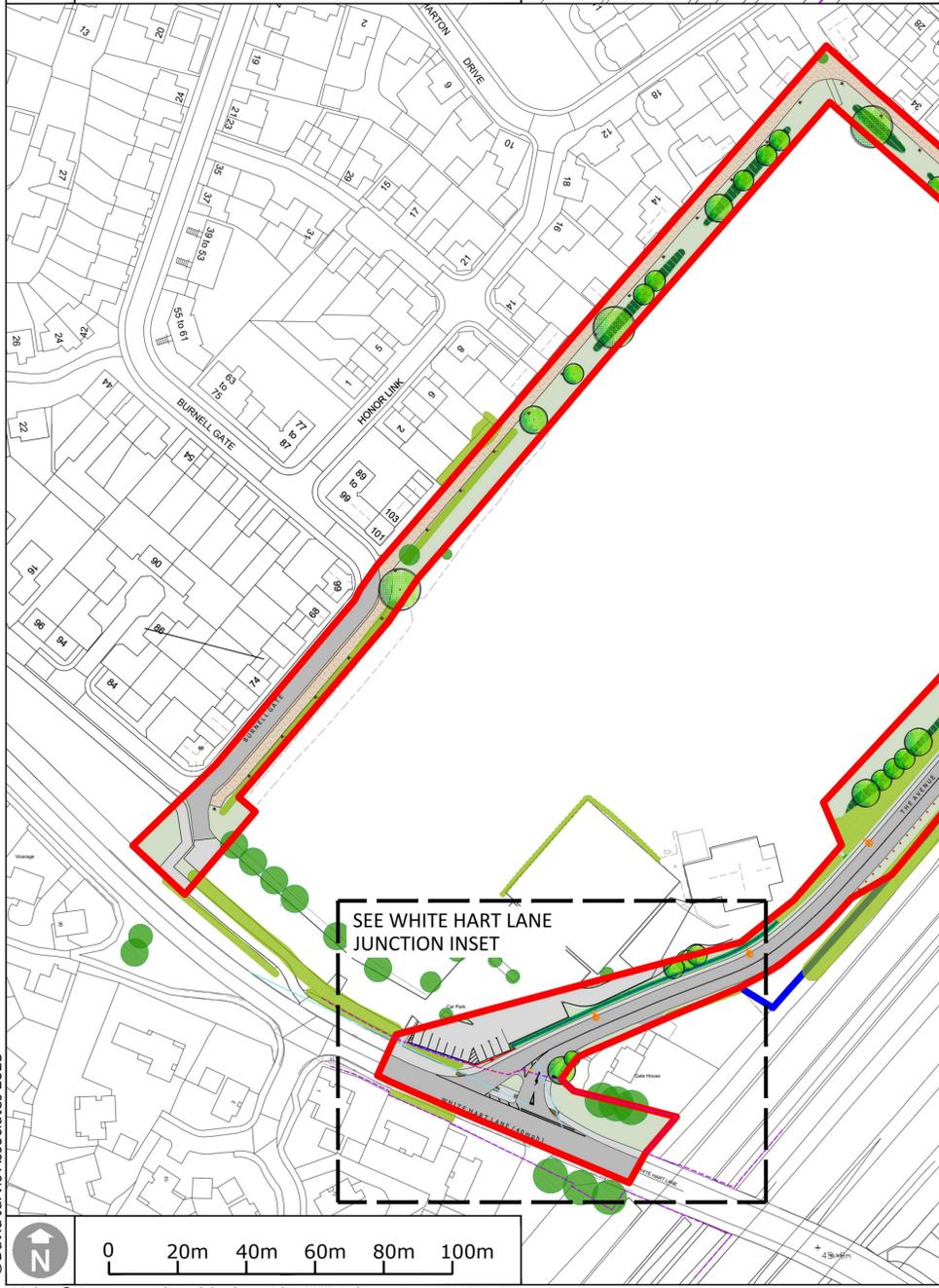
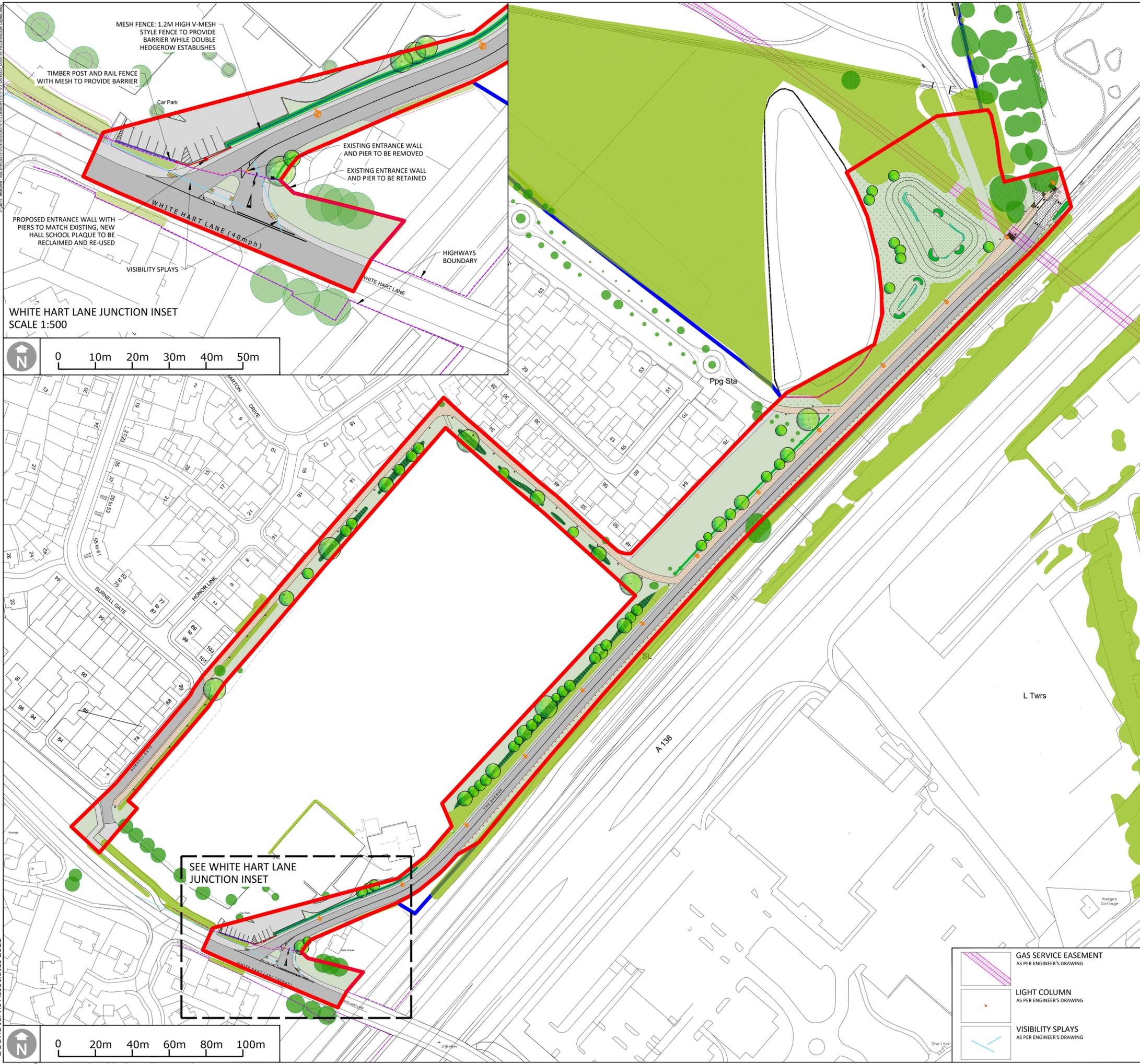
scale: 1:500 @ A0 drawn by: PB checked by: IM

date: AUGUST 2021 cad file: MASTER

title: PROPOSED BUS LINK GENERAL ARRANGEMENT DRAWING

Mayer Brown Limited jobcode: CPBEAULIEU.1 suitability: revision: P13

drawing number: Z03100_MAY_NA_NA_SK_D_1001



KEY

- APPLICATION SITE
- SITE WIDE BOUNDARY; AS PER OUTLINE APPLICATION
- EXISTING TREES TO BE RETAINED
- EXISTING HEDGEROW AND VEGETATION TO BE RETAINED
- TREES FOR DETAILS SEE PLANTING PLANS
- PROPOSED NATIVE THICKET PLANTING FOR DETAILS SEE PLANTING PLANS
- NATIVE FIELD HEDGE INFILL GAPS ALONG CARRIAGE DRIVE FOR DETAILS SEE PLANTING PLANS
- EXISTING CAR PARK PLANTING TO BE RETAINED
- SHRUB PLANTING- HEADWALLS FOR DETAILS SEE PLANTING PLANS
- MARGINALS FOR DETAILS SEE PLANTING PLANS
- SPECIES RICH MEADOW GRASSLAND - SHORT FOR DETAILS SEE PLANTING PLANS
- SPECIES RICH MEADOW GRASSLAND - LONG FOR DETAILS SEE PLANTING PLANS
- EXISTING POND
- ROAD TARMAC TO ENGINEER'S SPECIFICATION
- SPEED TABLE ASPHALT SURFACE TO ENGINEER'S SPECIFICATION
- BUS GATE ASPHALT SURFACE TO ENGINEER'S SPECIFICATION
- CYCLEWAY CROSSING ASPHALT SURFACE TO ENGINEER'S SPECIFICATION
- FOOTPATH TARMAC TO ENGINEER'S SPECIFICATION
- PRIMARY CYCLEWAY AND FOOTPATH ADDGRIP; TERRABASE RUSTIC RESIN BOUND POROUS PAVING, RUSTIC GOLD, 6MM, TIMBER EDGE
- EXISTING CHAIN LINK FENCE LOCATE AROUND EXISTING POND
- MESH FENCE V-MESH STYLE SECURITY FENCE, 1.2M HIGH, COLOUR GREEN
- EXISTING ENTRANCE WALL AND PIER TO BE RETAINED
- PROPOSED WALL TO MATCH EXISTING ENTRANCE WALL TO ENGINEER'S SPECIFICATION
- EXISTING ENTRANCE WALL AND PIER TO BE REMOVED
- TIMBER LOCKABLE BOLLARDS WOODSCAPE; REMOVABLE HARDWOOD TIMBER BOLLARDS
- SMARTSCAPE SOLAR BOLLARD ZETA SOLAR WAYFINDING SYSTEM SMARTSCAPE SOLAR BOLLARDS, TIMBER FINISH, 1000MMX140MMX140MM, SPACING TO ENGINEER'S REQUIREMENT
- LOW LEVEL TIMBER BOLLARDS TIMBER BOLLARDS WITH REFLECTIVE STRIP, LOCATION MAYER BROWN DRAWING 'PROPOSED BUS LINK GENERAL ARRANGEMENT DRAWING 203100_MAY_NA_NA_SK_D'
- HIGHWAYS BOUNDARY AS PER ENGINEER'S DRAWING
- GAS SERVICE EASEMENT AS PER ENGINEER'S DRAWING
- LIGHT COLUMN AS PER ENGINEER'S DRAWING
- VISIBILITY SPLAYS AS PER ENGINEER'S DRAWING

Notes

Related Drawings: DIA Drawing based on

- Mayer Brown Bus Link layout, reference 1001 Rev P13, revised 16/07/2022
- AECOM constraints and drainage strategy, reference DRX-ACM 203100_AEC_NA_NA_DK_C_5344, received 14/07/2022
- Network Rail station layout, reference 150796-JMS-DRG-EAR-001010, received 21/07/2022

Issue: Drawn by David Jarvis Associates Limited (CROWN COPYRIGHT). ALL RIGHTS RESERVED 2023 LICENCE NUMBER 01000313. This drawing is for Planning purposes only - Do not use this drawing for Construction. The information contained in the drawing should be used as a guide to the final forms and finishes of the landscape scheme. Any revisions to be approved by the Client and Local Authority

Scaling: Do not scale this drawing. Use given dimensions only.

Setting out: refer to Engineers for information regarding setting out. In the event of discrepancy refer to Engineers in the first instance.

Survey: Original survey provided by the Client.

Services: Where possible these are identified on the drawings but, for the avoidance of doubt all services/utility locations should be considered indicative until identified on site. To ensure those services / utilities shown are current refer to the original survey provider or utilities designer or Client for confirmation and further information regarding easements. In the event of new services being installed refer to the appointed Engineer. It is recommended that hazard warning tape 'danger electric cable/danger services' to be installed over all service routes (to remain on site) to current BS guidelines (BS7671).

Lighting: Refer to lighting engineers drawings.

Planting: Plant species are selected and located in line with consideration of the site conditions, NHIC guidelines and discussions with the Local Authority and design team. All plants and planting procedure to conform to the David Jarvis Associates Limited Landscape Specification that will accompany the Construction Issue drawings. No species or plant location is to be varied without prior consent of the Landscape Architect.

Biosecurity: All plant stock to be sourced from a supplier certified to be pest and disease free and in accordance with Plant Passport / Animal and Plant Health Agency (APHA) and current DEFRA requirements. Supplier information / certification to be retained for a period of not less than 12 years and must be made available upon request.

Foundations: Developers / Contractors to ensure that all foundations (buildings and external walling) are designed and constructed so as to take into account, at the time of maturity, any existing or proposed trees, hedgerows or other vegetation on the application site or existing vegetation on land adjoining the site at the time of construction and any trees felled or hedgerows removed on or adjacent to the site during the previous 15 years. For this purpose the developer / contractors will submit all relevant details to the authority dealing with the Building Regulations Certificate.

Design Levels: Refer to Engineers where design levels are not shown.

CDM: Drawings to be read in conjunction with Designers risk assessment. Potential risks above that of those associated with the general construction typical to the drawing are identified below;

Drawing Revision

Rev.	Date	Description	Drawn	Checked
P8	18/05/2023	Bollards now shown and keyed in.	ED	ED
P7	21/03/2023	Fence by car park reverted to v-mesh style security fence, planting added by northern bus gate.	ED	ED
P6	02/12/2022	Illuminated bollards added along the footpath around the existing pitches.	ED	PG
P5	22/11/2022	Amended following client comments.	ED	PG
P4	16/11/2022	Amended following client comments.	ED	PG
P3	10/08/2022	Amended following client comments.	ED	PG
P2	08/08/2022	Revised to include retained entrance wall and fence/hedge arrangement alongside car park.	ED	PG
P1	22/07/2022	First issue.	ED	PG

Status: **PLANNING**

DAVID JARVIS ASSOCIATES

DAVID JARVIS ASSOCIATES LIMITED
1 Tennyson Street Swindon Wiltshire SN1 5DT
t: 01793 612173
e: mail@davidjarvis.biz
w: www.davidjarvis.biz

Client: **COUNTRYSIDE PROPERTIES (UK) LTD.**

Project: **BEAULIEU BUS LINK**

Drawing Title: **LANDSCAPE LAYOUT**

Scale: 1:1000	Sheet Size: A1	Date Plotted: 18/05/2023
Client Ref.: -	Drawing Ref.: 3075-5-2	Drawing No.: DR-0002
		Version: 54-P8

OS data © Crown copyright and database rights 2023 Ordnance Survey 0100031673

© David Jarvis Associates 2023